

# Middleton Township Resurfacing Project

## Ovitt Road (T-104) ~ SR 65 to SR 64 and Reitz Road (T-101) ~ SR 65 to SR 25

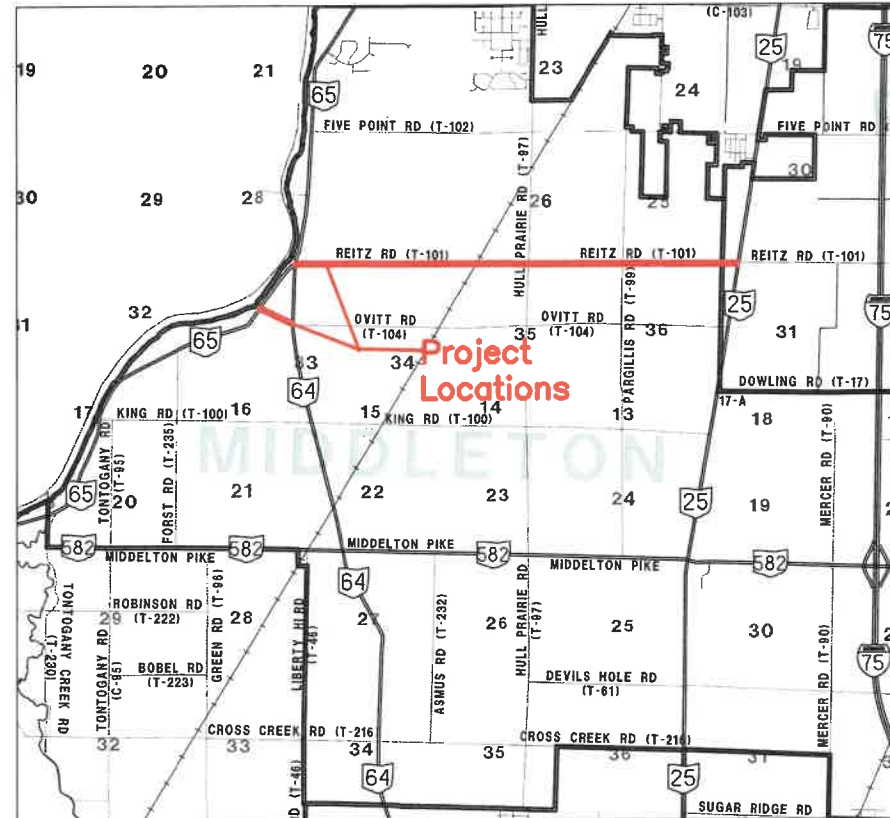
Middleton Township, Wood County, Ohio

### Conventional Symbols

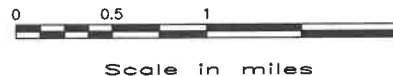
- Section Line
- Corporation Line
- Fence Line
- Centerline
- Right of Way (Ex)
- Temporary Right of Way
- Utility Easement
- Guardrail
- Construction Limits
- Edge of Pavement (Ex)
- Edge of Pavement (Prop)
- Edge of Shoulder (Ex)
- Edge of Shoulder (Prop)
- Ditch / Creek
- Tree Line
- Ownership Hook
- Property Line
- Break Line
- Tree (Ex) Shrub (Ex)
- Tree (Remove) Shrub (Remove)
- Evergreen (Ex) Stump
- Evergreen (Remove) Stump (Remove)
- Wetland (Prop) Grass (Prop) Aerial Target
- Post Mailbox Fire Hydrant
- Guy Wire Telephone Line Marker TEL, Telephone Riser
- Water Valve Water Meter
- Utility Valve Unknown Light Pole
- Telephone Pole Power Pole

### Survey Legend

- |                           |                           |
|---------------------------|---------------------------|
| <b>Found</b>              | <b>Set</b>                |
| R/W Monument Box          | R/W Monument Box          |
| Concrete Monument         | Concrete Monument         |
| Railroad Spike            | Railroad Spike            |
| I.P.F. Iron Pin           | I.P.S. Iron Pin W/ ID Cap |
| I.P.F. Iron Pin W/ ID Cap | P.S. Iron Pipe            |
| P.F. Iron Pipe            | P.K.S.P.K. Nail           |
| P.K.F. P.K. Nail          |                           |



Location Map



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### Project Description

This project is in Middleton Township and consists of overlaying the existing paved surface of Ovitt Road from State Route 65 to State Route 64 and Reitz Road from State Route 64 to State Route 25.

### 2023 Specifications

The standard specifications of the State of Ohio, Department of Transportation, including changes and supplemental specifications listed in the proposal, and the Wood County Construction Standards shall govern this improvement.

### Approvals

I hereby approve these plans and declare the making of this improvement will require the closing of the highway to traffic and detours will be provided by the Middleton Township Trustees.



*John M. Musteric*  
John M. Musteric, P.E., P.S.  
Wood County Engineer

*17 APR 2024*  
Date

We, the Trustees of Middleton Township in formal session, hereby approve these plans and certify the necessary right-of-way is available. We agree to maintain the project in a manner satisfactory to the Director of Transportation of the State of Ohio, and/or his duly authorized representative and will make ample provisions each year for such maintenance.

Performed under the authority of Section 5555.02 Et. Seq. and Section 5535.01 Et. Seq. of the Ohio Revised Code.

Board of Middleton Township Trustees

*4-17-24*  
Date

*Donald Cromley*  
Donald Cromley, Trustee

*Mike Moulton*  
Mike Moulton, Trustee

*Fred Vetter*  
Fred Vetter, Trustee

UNDERGROUND UTILITIES  
CONTACT TWO WORKING DAYS  
BEFORE YOU DIG

OHIO811, 8-1-1, or 1-800-362-2764  
(Non members must be called directly)

Supplemental Prints of Standard Construction Drawings			
O.D.O.T. DRWG.	DATE	O.D.O.T. DRWG.	DATE
BP-3.1	1-21-22	MT-101.60	4-21-23
DM-1.1	7-17-20	MT-105.10	1-17-20
HW-2.2	7-20-18		

Supplemental Specifications	
832	7-21-23

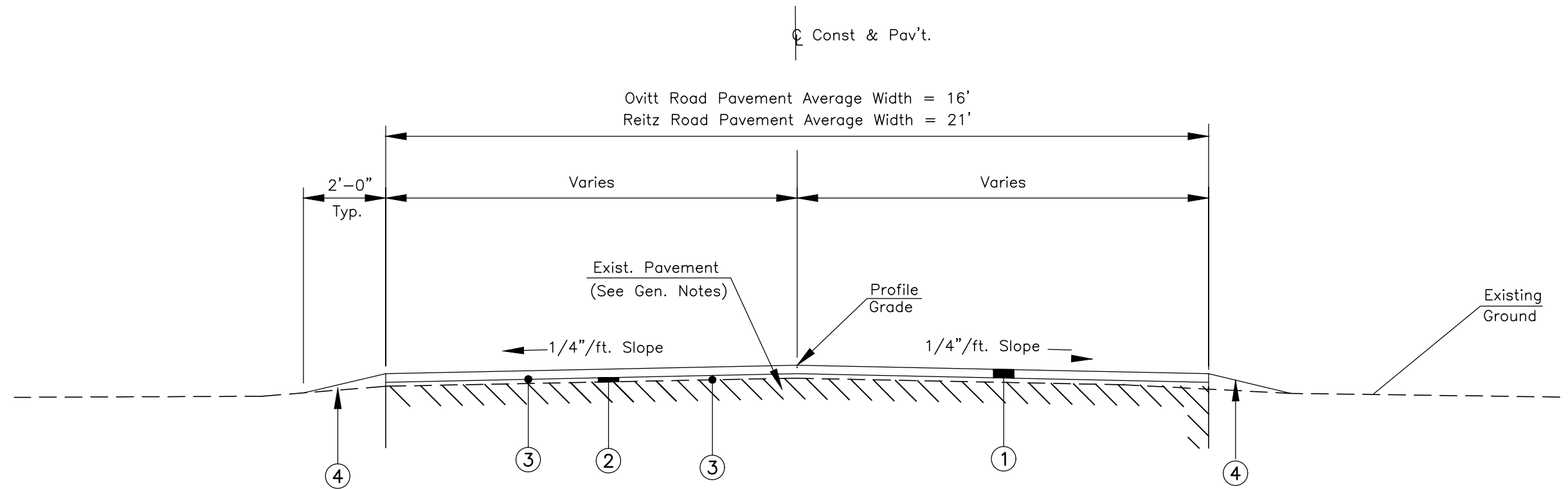
TITLE

WOOD COUNTY ENGINEER'S OFFICE  
John M. Musteric, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED JPS  
DRAWN SPJ  
REVIEWED JPS  
APPROVED JMM  
JOB NUMBER  
REVISION

MIDDLETON TOWNSHIP RESURFACING PROJECT  
WOOD COUNTY

1/11



**TYPICAL SECTION**

OVITT ROAD ~ STA. 10+47 TO STA. 24+91 = 1444 LIN. FT. (0.273 MILES)  
 REITZ ROAD ~ STA. 21+00 TO STA. 95+76 = 7476 LIN. FT. (1.416 MILES)  
 REITZ ROAD ~ STA. 96+48 TO STA. 117+36 = 2088 LIN. FT. (0.395 MILES)  
 REITZ ROAD ~ STA. 118+95 TO STA. 203+50 = 8455 LIN. FT. (1.601 MILES)  
**TOTAL = 19463 LIN. FT. (3.685 MILES)**

**LEGEND**

- ① Item 441: Asphalt Concrete, Surface Course Type-1, (1.0") (448) PG64-22
- ② Item 441: Asphalt Concrete, Intermediate Course Type-1, (0.50" Min Leveling) (449)
- ③ Item 407: Non-Tracking Tack Coat
- ④ Item 411: 2" (Avg.) Stabilized Crushed Aggregate (2' Wide)

**PAVEMENT RESURFACING**

Pavement areas designated for resurfacing shall be prepared and treated in accordance with Item 407 and Item 408. Feathered areas shall be placed in accordance with O.D.O.T. Std. Constr. Dwg. "BP-3.1".

**FULL WIDTH PAVEMENT PLACEMENT**

The leveling and surface courses shall each be placed in a single full width pass.

DATE: 4/17/2024 10:45 AM FILE: P:\DRAWINGS\ROADWAY\2024\MIDDLETON TWP RESURFACING\2024\MIDDLETON TWP RESURFACING\_TYPICAL.DWG

## GENERAL

### ENGINEER

All references to the State, the Director, the Engineer or the Inspector in the "State of Ohio, Department of Transportation, Construction and Material Specifications" (ODOT-CMS) manual shall be considered references to the Wood County Engineer or his duly authorized representative.

### REGULATIONS

All work must comply with applicable federal, state, and local regulations in all respects, including compliance with the Occupational Safety and Health Administration regulations.

### BEGINNING CONSTRUCTION

The Engineer shall be notified seven (7) days prior to the beginning of the actual construction.

### PROFILE

The profile grades as shown on the plans are on the centerline of construction.

### ROUNDING

The rounding at slope breakpoints shown on the typical sections apply to all cross sections even though otherwise shown on the plans.

### SURVEY HORIZONTAL & VERTICAL CONTROL

The Wood County Engineer's office will establish horizontal and vertical control for the project.  
Contact PH: 419-354-9060

### CONTINGENCY QUANTITIES

The Contractor shall not order materials or perform work for items designated by plan note to be used "as directed by the Engineer" unless authorized by the Engineer. The actual work locations and quantities used at the discretion of the Engineer shall be made a matter of record by incorporation into the final change order governing the completion of this project.

### INSPECTION & APPROVAL

All work performed within the scope of this project shall be subject to inspection and the approval of the Wood County Engineer or his duly authorized representative.

### ESTIMATED QUANTITIES

The estimated quantities are for the purpose of competitive bidding and are not necessarily the final pay quantities. Final field measurements and calculations shall determine the final quantity to be paid.

### CONCRETE SURFACE FINISH

The requirements of ODOT-CMS Section 511 rubbed finish shall apply for all exposed concrete surfaces.

### CONSTRUCTION LAYOUT STAKES

The contractor shall be responsible for placing all construction layout stakes on the project. Wood County will provide control points for all baselines and bench marks shown on the plans.

### HAUL ROADS

The Contractor shall maintain haul roads in a condition acceptable to the Wood County Engineer. The maintenance shall include, but is not limited to, the removal of mud, stone, and other materials from pavement surfaces. Haul routes shall be approved by the Wood County Engineer prior to construction of this project.

### REVIEW OF DRAINAGE FACILITIES

Before any work is started on the project and again before the final acceptance by the County, representatives of the County Engineer and the Contractor shall make inspection of all existing sewers and ditches which are to remain in service and which may be affected by the work. Records of the inspection shall be kept in writing by the County.

All new conduits, catch basins, and manholes constructed as a part of the project shall be free of all foreign matter and in a clean condition before the projects will be accepted by the County.

All existing sewers and ditches inspected initially by the above mentioned parties shall be maintained and left in a condition reasonably comparable to that determined by the original inspection. Any change in the condition resulting from the Contractor's operations shall be corrected by the Contractor to the satisfaction of the Engineer.

Payment for all operations described above shall be included in the Unit Bid Price for the applicable Item 611.

### MAINTAINING DITCH FLOW

The Contractor shall so conduct his operations as to maintain the flow of ditches at all times. This work shall consist of all operations related to maintaining ditch flow including pumping, fluming, etc. The plans for the proposed maintenance shall be submitted to the Wood County Engineer for approval at least five (5) days before construction. The Contractor shall be responsible, however, for any damages caused by upstream flooding due to his operations. Payment for this work shall be included in the Unit Bid Price for the applicable Item 611.

### WORK AGREEMENTS

All work that is required outside of the limits of the public right-of-way shall be done under work agreements with the individual property owners affected. The Wood County Engineer will obtain said agreements, however, the Contractor shall ensure that said agreements have been procured before proceeding with the work.

### MITIGATING ENVIRONMENTAL IMPACTS

In order to minimize any adverse construction noise impacts, any power-operated construction type device shall not be operated during normal sleeping hours. In addition, any power-operated construction-type device shall not be operated in such a manner that the noise created substantially exceeds the noise customarily and necessarily attendant to the reasonable and efficient performance of such equipment. A temporary crossing or ford shall not be developed in conjunction with this project, either for the contractor's equipment or for vehicular traffic.

### MONUMENTS, BENCH MARKS, AND CONTROL POINTS

The Contractor shall, prior to actual construction, erect protective barricades around all visible survey monuments that are to remain in or adjacent to the construction limits. Any monument, bench mark, control point, property corner stake, pin, or marker damaged or disturbed by construction shall be replaced by the Wood County Engineer at the expense of the Contractor.

### SLAG

Slag materials shall not be used on this project.

## ROADWAY

### NON-RIGID PAVEMENT REMOVAL

Removal and disposal of existing non-rigid pavement shall be included in the Unit Bid Price for Item 203 - Excavation, not including embankment.

### SPOILS MATERIALS

Disposal of all spoil materials shall be the responsibility of the Contractor. All spoil materials shall be hauled away from the site and disposed of in a manner suitable to the Engineer. The cost of this work shall be included in the Unit Bid Price for the pertinent item.

### EXCAVATED MATERIALS

Surplus earth shall be spread over the project site and/or adjacent properties "as directed by the Engineer" and payment for this work shall be included in the Unit Bid Price for Item 203 - Embankment. Unusable subgrade material shall be removed and disposed of in a manner suitable to the Engineer and payment for this work shall be included in the Unit Bid Price for Item 203 - Excavation, not including embankment.

### RECLAIMED ASPHALT CONCRETE PAVEMENT

In areas of embankment construction where pavement exists, the existing pavement shall be reclaimed in accordance with ODOT-CMS 703.16-Suitable Materials for Embankment Construction. Payment for Reclaimed Asphalt Concrete Pavement shall be included in the Unit Bid Price for Item 203-Embankment.

### CLEARING AND GRUBBING

Although there are no trees and/or stumps specifically marked for removal within the limits of this project, a lump sum quantity has been included in the General Summary for Item 201, Clearing and Grubbing. All provisions as set forth in the specifications under this item shall be included in the bid price for Item 201, Clearing and Grubbing.

### DRIVEWAYS

Driveways damaged or altered during project construction shall be replaced or repaired as per the existing driveway. Payment for this work shall be included in the Unit Bid Price for Item 203 - Excavation, not including embankment.

## PAVEMENT

### AGGREGATE BASE, AS PER PLAN

Materials furnished for Item 304 - Aggregate Base shall exclude the use of all slags.

### PAVEMENT JOINTS

Where the new construction meets the existing pavement a neat saw-cut joint shall be made prior to removal of the existing pavement. Payment for this work shall be included in the Unit Bid Price for Item 441 - Asphalt Concrete.

### ASPHALT CONCRETE PAVEMENT

The Asphalt Concrete Pavement shall consist of constructing a Surface Course (Type-1) in accordance with ODOT-CMS Item 441. The design criteria for Medium Traffic Volumes shall be used unless otherwise specified.

### PAVEMENT PREPARATION

The existing pavement surface shall be cleaned and maintained free of accumulated material in accordance with ODOT-CMS section 401.06.

### PAVEMENT REPAIR

Pavement repair work shall consist of spot leveling and/or patching operations applied in advance of the leveling course. All work shall be in accordance with Item 253 - pavement repair and "as directed by the engineer".

### LEVELING COURSE APPLICATION

The asphalt paver shall be equipped with a forty (40) foot longitudinal beam when applying the leveling and surface courses. All longitudinal joints shall be staggered between courses.

### TACK COAT

Tack coat and cover aggregate operations shall be in accordance with ODOT-CMS Item 407. The rate of application of the tack coat shall be subject to adjustment "as directed by the engineer". Plan quantities indicate an average application rate of 0.08 gallons per square yard of tack coat for estimating purposes only. The payment for cover aggregate shall be included in the unit bid price for Item 407 - Tack Coat.

### ASPHALT REJUVENATING AGENT, APP

The asphalt rejuvenating agent shall be an emulsion composed of a petroleum resin oil base uniformly emulsified with water. Each bidder must submit with his bid a certified statement from the asphalt rejuvenator manufacturer showing that the asphalt rejuvenating emulsion conforms to the required physical and chemical requirements. The rejuvenating agent shall have a record of at least five years of satisfactory service as a petroleum maltene based emulsion asphalt rejuvenating agent and in-depth sealer. Satisfactory service shall be based on the capability of the material to decrease the viscosity of the asphalt binder by petroleum maltene fraction replacement method, increase the phase angle and provide an in-depth seal.

The asphalt rejuvenating agent shall be applied to the asphalt surface prior to placement of pavement markings. The contractor must allow a minimum of 7 days between asphalt rejuvenating agent application and placement of final pavement markings.

GENERAL NOTES



WOOD COUNTY ENGINEER'S OFFICE  
John M. Musters, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED  
JPS

DRAWN  
SPJ

REVIEWED  
JPS

APPROVED  
JMM

JOB NUMBER  
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REVISION  
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MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY

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11

## MAINTAINING TRAFFIC

### DETOURS

Roads may be closed to thru traffic "as directed by the Engineer". However, the Contractor shall maintain access to local residences at all times. The Contractor shall notify the Wood County Engineer in writing forty-eight (48) hours before any detour or rerouting of traffic is put into effect.

County Office Building  
One Courthouse Square  
Bowling Green, Ohio 43402  
Phone: (419) 354-9060

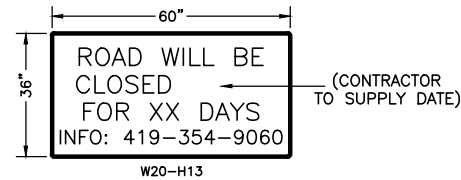
Plan Designated Detour Route:

Shall be submitted by the contractor prior to any construction taking place and shall be approved by the Wood County Engineer and/or Township Trustees.

### BARRICADES, LIGHTS, AND SIGNS

The Contractor shall maintain full width access to all driveways at all times. Notice of closure signs, as detailed in these plans, shall be erected by the contractor at least one week in advance of the scheduled road closure. The signs shall be erected on the right-hand side of the road facing traffic. They shall be placed so as not to interfere with the visibility of any other traffic control signs. They should be erected at the point of closure.

Notice of closure sign:



The Contractor shall provide, erect and maintain standard 48 X 30 inch road closed signs, sign supports, barricades and lights, as detailed in SCD MT-101.60 at "Begin Work" and "End Work" stations of the project during the period in which the affected roads are closed to traffic.

Payment for any additional signs and/or barricades required to provide clarity to the traffic control schemes set forth in the plans or the OMUTCD, or payment for any signs and/or barricades which require relocation to provide clarity as determined by the engineer, shall be at no additional cost to the county and shall be considered incidental to and included in the lump sum contract price for Item 614 - Maintaining Traffic.

### MAINTAINING TRAFFIC ON AFFECTED ROADS

Methods of maintaining traffic shall be in accordance with item 614 and the OMUTCD.

The contractor shall submit to the Wood County Engineer a description of the proposed sequence of construction operations for approval, prior to starting the work. The sequence of construction shall reflect absolute minimum durations for temporary traffic lane use.

When necessitated by construction, one way traffic flow may be established. A traffic lane at least ten (10) feet wide shall be provided with traffic flow controlled by at least two (2) flaggers. Traffic control devices shall be placed to limit traffic to one lane in accordance with the OMUTCD and ODOT standard drawings in general and MT-97.10 and MT-97.12 specifically. In no case will one way flow be permitted until adequate traffic control devices and flaggers are in place and approved by the Engineer. The one way traffic operation shall be limited to daylight hours only. The road shall be reopened to two way traffic flow at night and at all times when the Contractor is not working such as weekends, holidays or when work is suspended due to weather conditions. All work and materials associated with the closure of roads affected by this project and only allowed during construction periods shall be paid for under Item 614 - Maintaining Traffic.

It is the intent to minimize the impact to the traveling public. Lane closures or restrictions are only allowed during the setting of the precast box culvert. Any other time during construction shall not be permitted. The level of utilization of maintenance of traffic devices shall be commensurate with the work in progress.

All other work and traffic control devices shall be in accordance with CMS 614 and other applicable portions of the specifications, as well as the OMUTCD. Payment for all labor, equipment and materials shall be included in the contract price for Item 614 - Maintaining Traffic, unless separately itemized in the contract documents.

Traffic control devices shall be set up prior to the start of construction and shall be properly maintained during construction. They shall remain in place only as long as they are needed and shall be immediately removed thereafter. All signs with messages which do not apply during a certain period shall be covered or set aside out of view of traffic. All traffic control devices shall be kept in proper position, clean, legible and in good working condition at all times.

Driveway access shall be maintained at all periods of construction by use of existing and proposed pavement, berms or shoulders to the satisfaction of the Engineer. The contractor shall provide residents and/or businesses with a minimum forty-eight (48) hour notice when access to their driveways will be restricted due to construction.

## CONDUIT

### CONDUIT

All conduit on this project shall be Reinforced Concrete Pipe (C-76, Class-3) in accordance with ODOT-CMS Section 706.02 or Polyvinyl Chloride (PVC), ASTM designations D-3034, SDR-35, in accordance with ODOT-CMS Section 707.45.

### CONDUIT BACKFILL

All conduits inside the paved area shall be backfilled vertically to the top of the trench and laterally to five (5) feet outside the edge of pavement with Low Strength Mortar (LSM) backfill. All conduits outside the paved area shall be backfilled with earth material and shall be graded, seeded and mulched upon completion of this project. Payment for this work shall be included in the Unit Bid Price for applicable Item 611.

### CONDUIT BEDDING

All conduit on this project shall be laid on Class "B" Bedding. Payment for granular bedding shall be included in the Unit Bid Price for the applicable Item 611.

### CONDUIT ENDS

All conduit, whether terminating in headwalls or endwalls, shall begin and end with pipe ends as normally fabricated by the manufacturer. Ends shall not be cut to fit either skew or slope.

### FABRICATION OF CONNECTIONS

When connecting a smaller pipe by breaking into an existing large pipe culvert, the lead pipe shall be trimmed to fit the inside circumference of the large pipe. Payment for this work shall be included in the Unit Bid Price for the applicable Item 611.

### EXISTING PIPES AND UTILITIES

Where the plans provide for a proposed conduit to be connected to, or cross over or under an existing sewer or underground utility, it shall be the responsibility of the Contractor to locate the existing pipes or utilities, both as to line and grade, before starting to lay the proposed conduit.

If it is determined that the elevation of the existing conduit, or existing appurtenance to be connected, differs from the plan elevation or results in a change in the plan conduit slope, the Engineer shall be notified before starting construction of any portion of the proposed conduit which will be affected by the variance in the existing elevations.

If it is determined that the proposed conduit will intersect an existing sewer or underground utility if constructed as shown on the plan, the Engineer shall be notified before starting construction of any portion of the proposed conduit which would be affected by the interference with an existing facility.

Payment for all operations described above shall be included in the contract price for the pertinent Item 611.

## UTILITIES

### UNDERGROUND UTILITIES

The locations of existing underground utilities shown on these plans are as obtained from the owners as required by Section 153.64 of the Ohio Revised Code. It is believed that they are essentially correct, but the exact locations shall be the responsibility of the Contractor. Wood County makes no guarantees as to their accuracy or completeness.

### UTILITY NOTIFICATION

The Contractor shall notify the Ohio Utility Protection Services (OUPS 800-362-2764) and the owners of underground utilities shown on the plans who are not members of a registered underground utility protection service at least two (2) working days (excluding Saturdays, Sundays, and legal holidays) prior to commencing construction operations in any area which may involve underground utilities.

### UTILITY STAKING

The owner of the underground utility shall, within two (2) days (excluding Saturdays, Sundays, and legal holidays) after notice is received, stake, mark, or otherwise designate the location of the underground utilities in the construction area in such a manner as to indicate their course together with the approximate depth at which they were installed. The marking or locating shall be coordinated to stay approximately two (2) days ahead of the planned construction in accordance with Sec. 153.64 of the Ohio Revised Code.

### UTILITY ADJUSTMENT

Any and all work required for Public or Private Utilities shall be done by and at the expense of the respective Owners, unless otherwise noted in these plans. The Contractor shall notify each respective utility company at least seven (7) days prior to the time of construction.

### UTILITY OWNERSHIP

The following utilities and owners are located within the limits of this project:

Cable/Telephone: Frontier 160 W. Wooster Street Bowling Green, OH 43402 Phone: (419) 354-9455 Attn.: Eric Massengill	Electrical: Hancock-Wood Electric 1399 Business Park Drive South North Baltimore, OH 45872 Phone: (419) 257-5034 Attn: Bob Hamlin
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Cable/Telephone:  
Century Link  
relocations@lumen.com



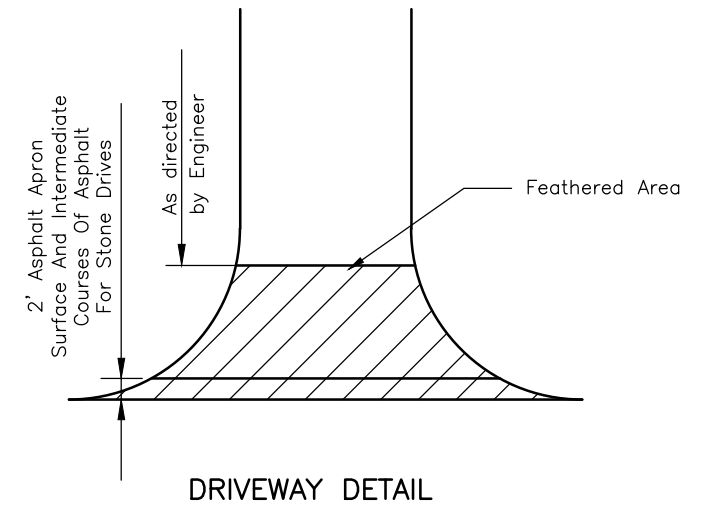
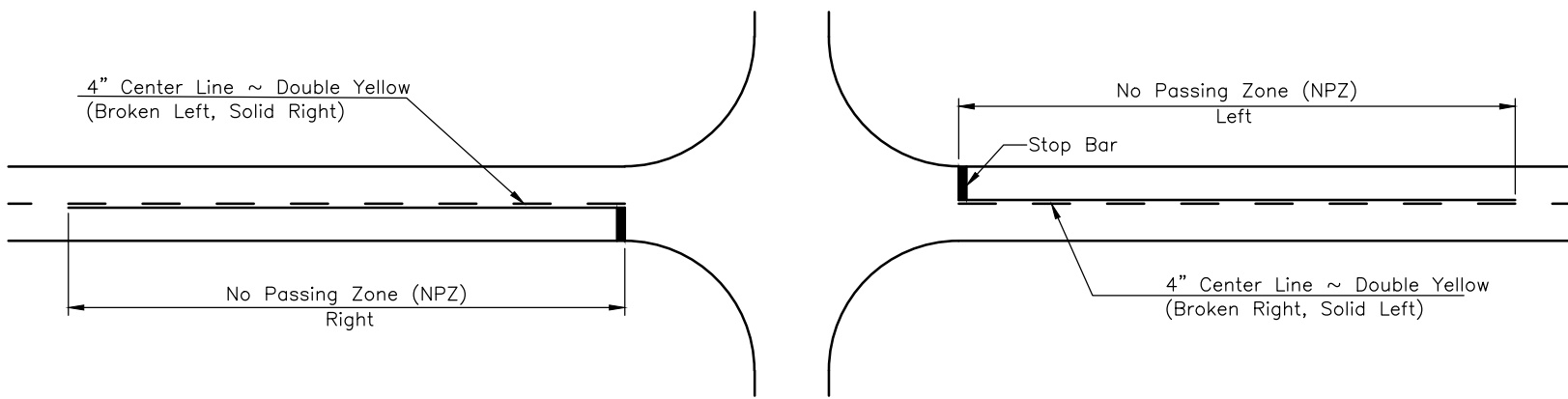
WOOD COUNTY ENGINEER'S OFFICE  
John M. Musters, P.E., P.S., County Engineer  
Bowling Green, Ohio

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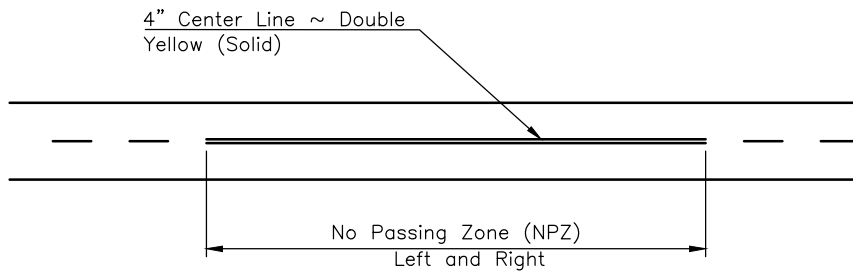
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RESURFACING PROJECT  
WOOD COUNTY

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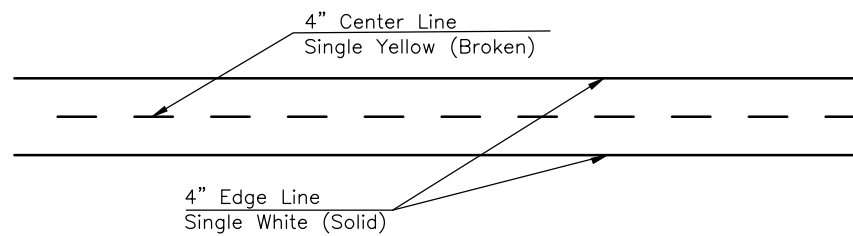


(ITEM 411 FOR STONE DRIVEWAYS)  
(ITEM 441 FOR ASPHALT OR CONCRETE DRIVEWAYS)

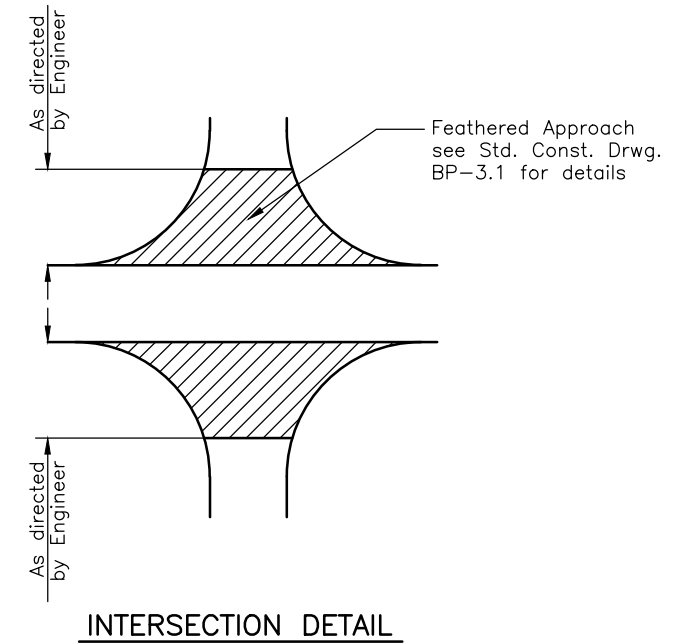


**NOTES**

Pavement Markings shall be applied in accordance with the "Ohio Manual of Uniform Traffic Control Devices" and as shown on this Sheet.



**TYPICAL PAVEMENT MARKINGS**



WOOD COUNTY ENGINEER'S OFFICE  
John M. Musteric, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED  
JPS

DRAWN  
SPJ

REVIEWED  
JPS

APPROVED  
JMM

JOB  
NUMBER

REVISION

MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY

## PAVEMENT SUMMARY

STATION		PROPOSED PAVEMENT		Average Width of Existing Pavement	Area of Proposed Pavement	254		407		411		441	
						Pavement Planing	Non-Tracking Tack Coat (.08 gal./s.y.)	Stabilized Crushed Agg.	0.50" Leveling Asphalt Conc Int. Course Type 1	1.0" Asphalt Conc Surface Course Type 1			
FROM	TO/AT	LENGTH Feet	WIDTH Feet	Feet	Sq.Yd.	Sq.Yd.	Gal.	Cu. Yd.			Cu. Yd.	Cu. Yd.	
Ovitt Road													
10+47	24+91	1444	16	16	2567		411	37			54	72	
Reitz Road													
21+00	95+76	7476	21	21	17444		2791	184			364	485	
96+48	117+36	2088	21	21	4872		780	52			102	136	
118+95	203+50	8455	21	21	19729		3157	209			411	548	
Add for Driveways								50			20		
Add for Feathered Approaches						744	60	15					
Add for Bridge Decks						227	37						
<b>Totals to General Summary</b>						<b>971</b>	<b>7236</b>	<b>547</b>			<b>931</b>	<b>1261</b>	

## GENERAL SUMMARY

ITEM	QUAN.	UNIT	DESCRIPTION
<b>ROADWAY</b>			
411	547	C.Y.	Stabilized Crushed Aggregate
<b>PAVEMENT</b>			
254	971	S.Y.	Pavement Planing, Asphalt Concrete
407	7236	Gal.	Non-Tracking Tack Coat, as per plan
441	931	C.Y.	Asphalt Concrete Intermediate Course, Type 1, (0.50" Min) (449) (Leveling)
441	1261	C.Y.	Asphalt Concrete Surface Course, Type 1, (1.0") (448) PG64-22
SPECIAL	56	Foot	Sawing and Sealing Bituminous Concrete Joints
<b>PAVEMENT MARKINGS</b>			
642	3.41	Mile	Center Line, Type 1
642	6.82	Mile	Edge Line, Type 1
642	2	Each	Railroad Symbol
<b>GENERAL</b>			
614	1	Lump Sum	Maintaining Traffic
624	1	Lump Sum	Mobilization

### Item Special – Sawing and Sealing Bituminous Concrete Joints

#### 1) DESCRIPTION

This work shall consist of cutting and sealing transverse joints on the new bituminous concrete overlay of box beam bridges. Bituminous concrete joints shall be constructed directly over, and in line with, the existing underlying transverse abutment joint of the box beams.

#### 2) MATERIALS

The joint sealant shall meet the requirements of ODOT CMS Item 705.04, Joint sealants, Hot-poured, for Concrete and Asphalt Pavements.

Acceptable alternate materials are:

A Silicone sealant meeting Federal Specifications TT-S-001543A Class A (one-part silicone sealants) and TT-S-00230C Class A (one-component sealants), such as those manufactured by General Electric, Silicone Products Divisions, 4015 Executive Park Drive, Cincinnati, Ohio 45242 (513-243-1955) or Dow Corning, 400 Techn Center, Suite 103, Milford, Ohio 45150 (513-831-3586); or

Sof-Seal, a cold-applied, low-modulus, two-component polymeric compound horizontal sealant as manufactured by W.R. Meadows, Inc., P.O. Box 543, Elgin, Illinois 60121 (800-342-5976).

#### 3) CONSTRUCTION DETAILS

A) General: The contractor shall conduct his operation so that the cutting, cleaning and sealing of transverse joints is a continuous operation that will be performed as soon as practical after the paving, but no later than four (4) days after placement of the asphalt concrete surface course. Traffic shall not be allowed to knead together or damage the joint cut prior to sealing.

B) Cutting of Transverse Joints: The contractor shall saw or rout transverse joints to the dimensions shown in the details on this sheet. The cut joints shall lie directly above each box beam abutment joint.

The joint location shall be marked on the new asphalt surface with a chalk line, or by some other acceptable method, before cutting. Details of the method for locating and accurately marking the proposed cuts shall be subject to the approval of the Engineer prior to starting any surfacing or paving operations.

The blade or blades shall be of such size that the full width and depth of the cut can be made with one pass. Dry or wet cutting will be allowed. Joints shall extend the full width of the bridge.

C) Cleaning Joints: Dry sawed joints shall be thoroughly cleaned with a sufficient amount of compressed air to remove any dirt, dust, or deleterious matter. Wet sawed joints shall be washed clean of all cuttings by flushing with a jet of water and with other tools as necessary. After flushing, the joint shall be blown out with compressed air. When the surfaces are thoroughly clean and dry, and just prior to placing the joint sealer, compressed air having a pressure of at least 90 p.s.i. shall be used to blow out the joint and remove all traces of dust.

In the event of freshly cut joints become contaminated before they are sealed, they shall be recleaned of all foreign material by high pressure water jet.

D) Sealing Joints: The joint shall be thoroughly dry before the sealant is placed. After cleaning and drying, a bond-breaker material shall be applied to the bottom of the groove.

Hot-poured joint sealant material shall be heated in a kettle or melter constructed as a double boiler, with the space between the inner and outer shells filled with oil or other heat transfer medium. Positive temperature control and mechanical agitation shall be provided. Heating must be in strict accordance with the manufacturer's recommendation. Joint sealer material shall never be kept heated at the pouring temperature for more than four (4) hours and shall never be reheated. Sealer left in the applicator at the end of a day's work shall be removed and discarded.

Hot-poured sealant shall be applied immediately through a nozzle, which must project into the sawed joint, filling from the bottom up. The seal shall completely fill the joint in such a manner that, after cooling, the level of the sealer will not be higher than 1/8" below the pavement surface. Any depression in the cooled seal greater than 3/16" shall be brought up to the specified limit by further addition of the hot-poured sealant. Care shall be taken in the sealing of the joints so that the final appearance will present a neat, fine line.

The cold applied sealant materials (polyurethane, silicone, and polymeric compounds) shall be installed as per manufacturer's recommendations, except as modified by this drawing or as directed by the Engineer. The sealant shall be installed when the ambient temperature is 40 degrees F or higher. Traffic shall not be allowed on the joint for one hour after application of the sealant.

#### 4) METHOD OF MEASUREMENT

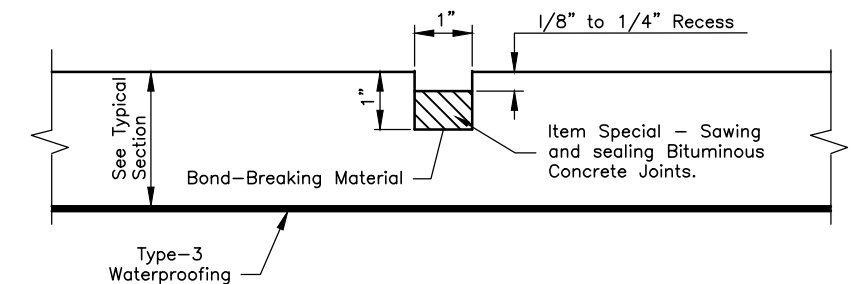
The quantity to be paid for under this item will be the number of linear feet of joints sawed and sealed as per the above requirements.

#### 5) BASIS OF PAYMENT

The unit price per linear foot for Item Special – "Sawing and sealing bituminous concrete joints" shall include the cost of all labor, materials, and equipment necessary to complete the work, including the furnishing and placing of the joint sealer material.

### ITEM 516 – 2" Deep Joint Sealer, as per plan

This item shall meet the material (Section 2) and sealing (Section 3D) specifications of Item Special – Sawing and sealing bituminous concrete joints.



### SEALING OF JOINTS AT ABUTMENTS

GENERAL SUMMARY  
AND SUBSUMMARIES



WOOD COUNTY ENGINEER'S OFFICE  
John M. Musteric, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED  
JPS

DRAWN  
JAB

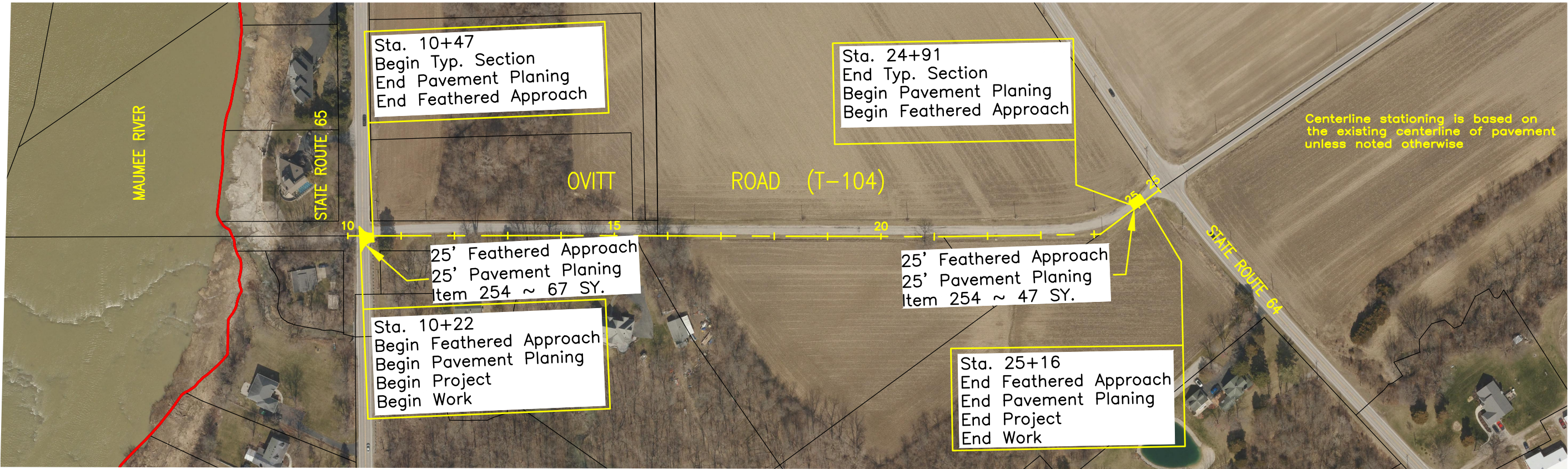
REVIEWED  
JPS

APPROVED  
JMM

JOB NUMBER  
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REVISION  
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MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY, OHIO



Sta. 10+47  
 Begin Typ. Section  
 End Pavement Planing  
 End Feathered Approach

Sta. 24+91  
 End Typ. Section  
 Begin Pavement Planing  
 Begin Feathered Approach

25' Feathered Approach  
 25' Pavement Planing  
 Item 254 ~ 67 SY.

25' Feathered Approach  
 25' Pavement Planing  
 Item 254 ~ 47 SY.

Sta. 10+22  
 Begin Feathered Approach  
 Begin Pavement Planing  
 Begin Project  
 Begin Work

Sta. 25+16  
 End Feathered Approach  
 End Pavement Planing  
 End Project  
 End Work

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WOOD COUNTY ENGINEER'S OFFICE  
 John M. Musteric, P.E., P.S., County Engineer  
 Bowling Green, Ohio

DESIGNED	JPS
DRAWN	SPJ
REVIEWED	JPS
APPROVED	JMM
JOB NUMBER	---
REVISION	---

MIDDLETON TOWNSHIP  
 RESURFACING PROJECT  
 WOOD COUNTY, OHIO



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WOOD COUNTY ENGINEER'S OFFICE  
John M. Musteric, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED	JPS
DRAWN	SPJ
REVIEWED	JPS
APPROVED	JMM
JOB NUMBER	---
REVISION	---

MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY, OHIO





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John M. Musters, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED	JPS
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APPROVED	JMM
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REVISION	---

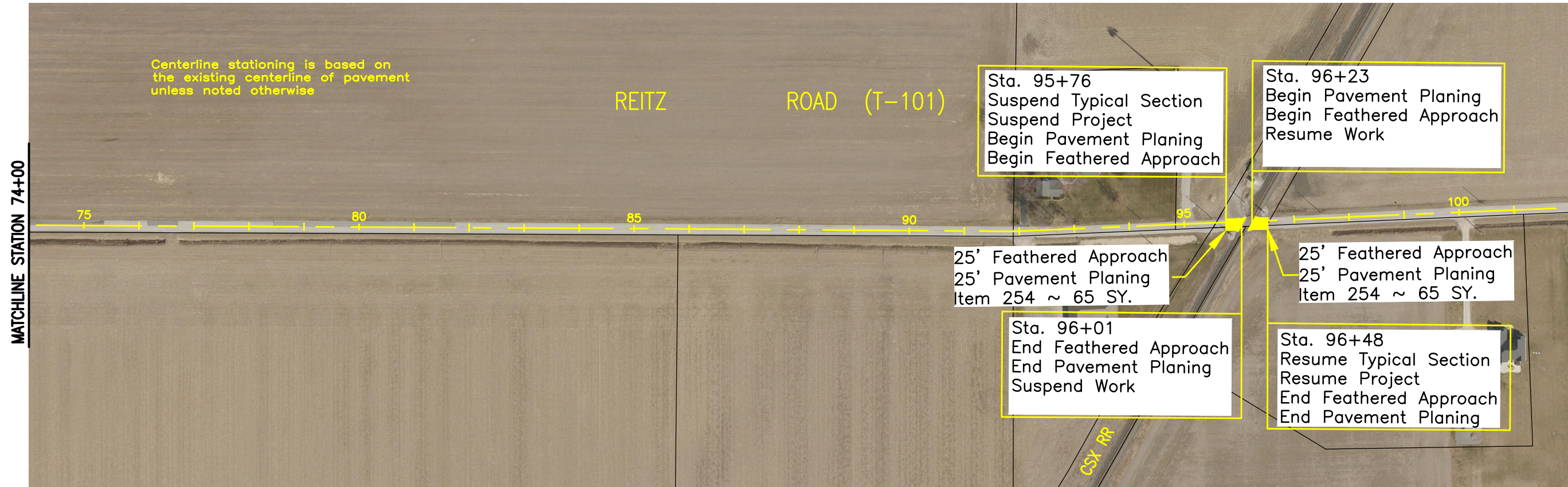
MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY, OHIO

Centerline stationing is based on the existing centerline of pavement unless noted otherwise

REITZ ROAD (T-101)

MATCHLINE STATION 74+00

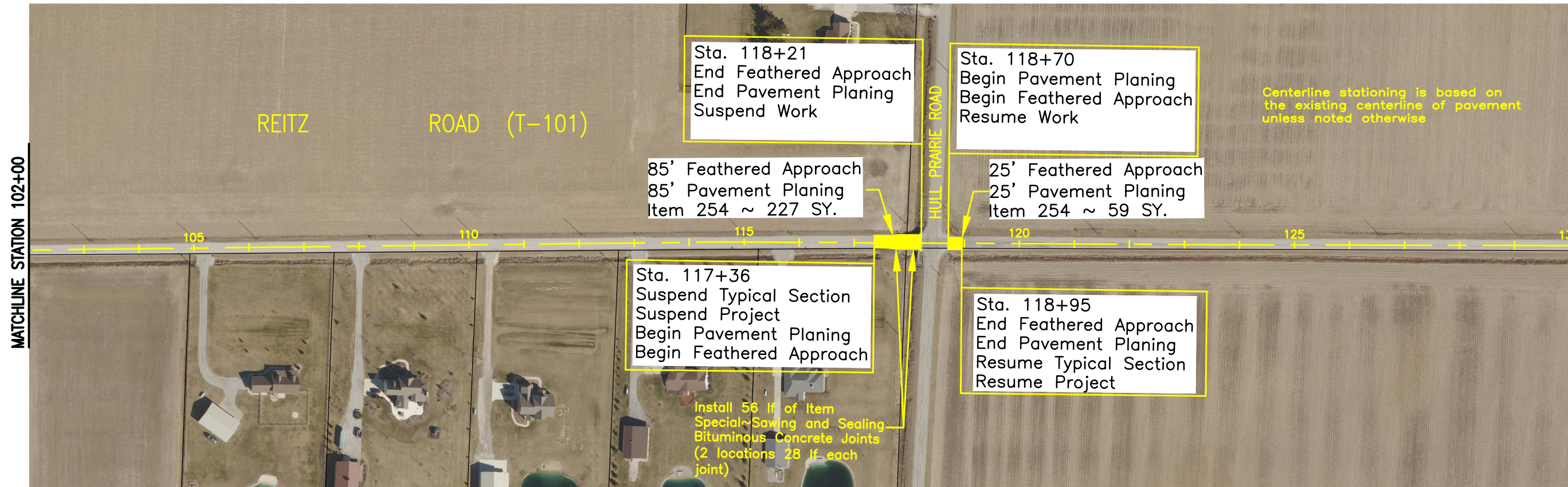
MATCHLINE STATION 102+00



MATCHLINE STATION 102+00

MATCHLINE STATION 130+00

REITZ ROAD (T-101)

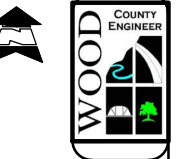


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SECTION 26, T1, USR, MIDDLETON TOWNSHIP

SECTION 25, T1, USR, MIDDLETON TOWNSHIP

SCALE: 1" = 200'

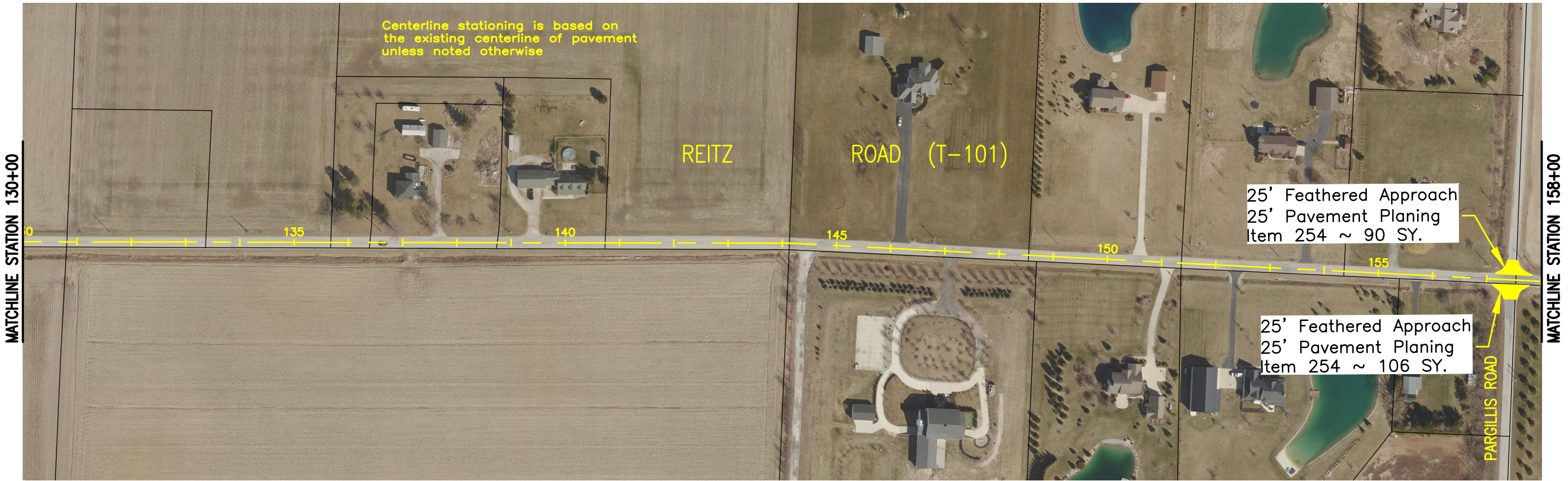


WOOD COUNTY ENGINEER'S OFFICE  
John M. Musteric, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED  
JPS  
DRAWN  
SPJ  
REVIEWED  
JPS  
APPROVED  
JMM  
JOB NUMBER  
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REVISION  
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MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY, OHIO

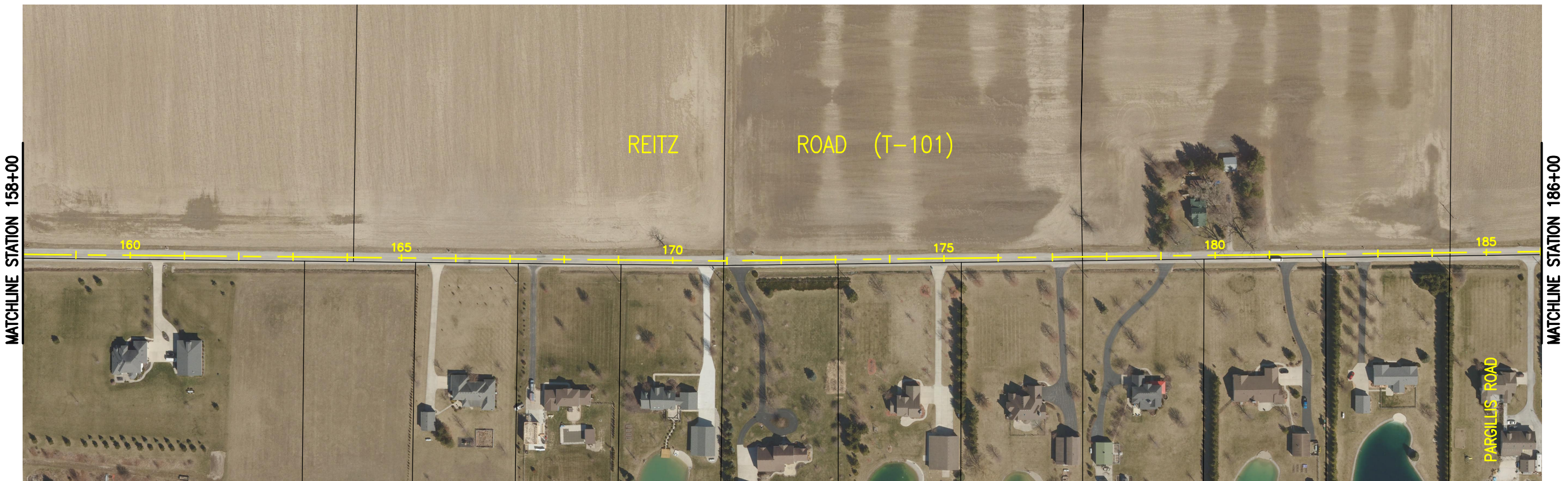
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SECTION 35, T1, USR, MIDDLETON TOWNSHIP

SECTION 36, T1, USR, MIDDLETON TOWNSHIP

SECTION 25, T1, USR, MIDDLETON TOWNSHIP



SECTION 36, T1, USR, MIDDLETON TOWNSHIP

SECTION 36, T1, USR, MIDDLETON TOWNSHIP

REITZ ROAD PLAN VIEWS  
STA 130+00 TO 186+00

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WOOD COUNTY ENGINEER'S OFFICE  
John M. Musteric, P.E., P.S., County Engineer  
Bowling Green, Ohio

DESIGNED  
JPS

DRAWN  
SPJ

REVIEWED  
JPS

APPROVED  
JMM

JOB NUMBER  
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REVISION  
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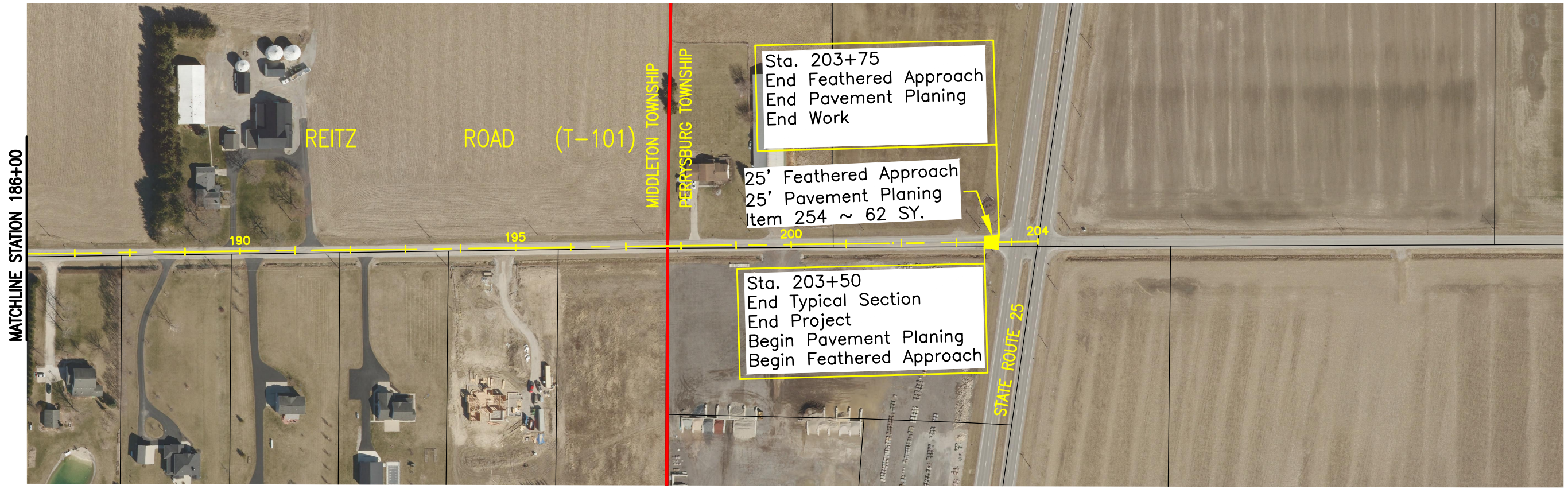
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MIDDLETON TOWNSHIP  
RESURFACING PROJECT  
WOOD COUNTY, OHIO

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MATCHLINE STATION 186+00

REITZ ROAD (T-101)

MIDDLETON TOWNSHIP

PERRYSBURG TOWNSHIP

STATE ROUTE 25